

# By-Product haulage guidelines

## 1. Overview

This guideline has been developed to provide a clear framework for the safe, legal, and responsible transport of forest by-products. It is designed to aid compliance with key regulatory requirements such as the Heavy Vehicle National Law (HVNL) and Chain of Responsibility obligations, while addressing occupational health and safety (OHS) standards.

The guideline also aims to minimise environmental impacts, reduce community disruptions, and maintain operational consistency across haulage activities. By setting out expectations for standards, driver responsibilities, operating hours, and emergency procedures, it supports risk management and promotes best practice in haulage operations.

## 2. Purpose

To ensure all haulage activities are conducted safely, legally, and with minimal environmental and community impact.

## 3. Scope

Applies to all haulage operations associated with forest by-product by buyers.

## 4. Legal & regulatory compliance

Operations are designed to maintain Chain of Responsibility (CoR) obligations for all parties and to comply with:

- Heavy Vehicle National Law (HVNL) for mass, dimension, fatigue, and load restraint.
- Log Haulage Industry Code of Practice.
- Vehicle configurations not subject to HVNL or not considered in the Log Haulage Industry Code of Practice used to transport By-Product must comply with DEECA Load Restraint and Transport Guidelines.
- Local council and VicRoads curfew requirements where applicable.

## 5. Operating hours for haulage in State forest

- **Standard Haulage Hours\*:**
  - Monday–Saturday: 6:00 am – 6:00 pm
  - No haulage on Sundays or public holidays unless approved.
- **Night Operations:**
  - Only permitted under exceptional circumstances as agreed upon by DEECA and in compliance with fatigue management obligations.

*\*Subject to conditions and operator availability*

## 6. Environmental & community considerations

- Avoid haulage during wet weather on unsealed roads to prevent road damage and sediment runoff.
- Minimise noise impacts by adhering to operating hours and using approved routes.
- Ensure haulage routes avoid sensitive areas (schools, residential streets) where possible.

## 7. Safety requirements

All vehicles must:

- Be roadworthy and inspected regularly.
- Comply with Log Haulage Industry Code of Practice or, (where not applicable), comply with DEECA Load Restraint and Transport Guidelines (RTG's) (see appendices 1-3).

Drivers must:

- Hold appropriate licenses.
- Maintain regular communication with all relevant parties (e.g. Operations personnel, haulage contractors, site supervisors).
- Confirm and adhere to pre-arranged schedules for loading and departure from loading sites.
- Notify promptly if delays or changes occur to avoid operational disruptions.

## 8. Documentation & records

The following records must be maintained by drivers:

- Vehicle inspections.
- Driver work/rest hours.
- Load restraint checks.
- Incident reports.

A docket must be completed for every load of by-product removed

## 9. Emergency procedures when hauling in State forest

In an emergency, always contact emergency services.

Reasonable attempts must be made by relevant parties to notify:

- Site supervisor
- Nominated DEECA contact
- Other parties scheduled to collect by-product

Any contingency routes outlined in the Operations Plan or otherwise provided by DEECA must also be utilised.

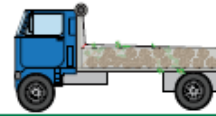
## 10. Compliance

- Vehicles or individuals which do not meet the Haulage Guidelines are unable to be loaded with by-product.
- Non-compliance with the Haulage Guidelines may result in the suspension of a truck or individual from being able to transport by-product.

## 11. Appendix 1: Light truck and tippers restraint and transport guideline



### Light Truck (Fixed or removable sides) and Tippers Restraint and Transport Guideline



#### Guideline:

This document provides guidance on minimising transport risk of forestry products for operations using light trucks with fixed or removable sides (carrying up to 8 tonne loads) in either round log form or as processed wood.

Transport of logs and other forest products (dependant on vehicle type) is also subject to the requirements of:

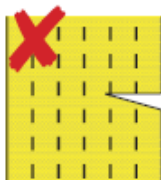
- Heavy Vehicle National Law Act 2018.
- Forestry Log Haulage Code of Practice March 2020. (Unprocessed timber and standard Haulage log trailers)
- Victorian Road Management Act 2004.

#### Load Restraint Equipment:

- ✓ 50mm or 75mm webbing, conforming to AS/NZS4380.
- ✓ 8mm minimum transport chains, tensioned with load binders, conforming to AS/NZS4344.
- ✓ Ausbinders, turnbuckle ratchet, web-dog binders or similar are a suitable alternative to dogs provided a minimum pre-tension of 750kg can be achieved.
- ✓ Cargo nets must be rated for the load weight, when used to restrain the load
- ✓ Equipment must be in good working order.
- ✓ Inspect the lashings for wear on a 12 monthly basis.
- ✗ Webbing straps worn more than 10% of their width should be discarded.
- ✗ Chains with excessively damaged, worn or bends links must not be used.
- ✗ Over center level binders (dogs) are a high risk and are not to be used.



Chain Wear



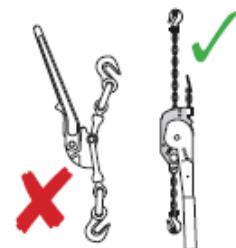
More than 10% wear



Stretched webbing



Standard Ratchet (push up)

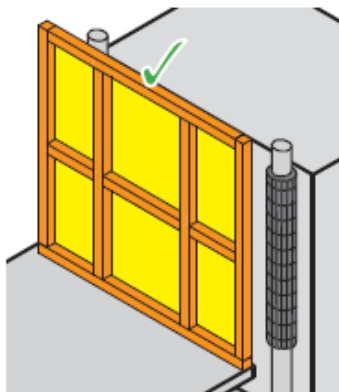


## Light Truck (Fixed or removable sides) and Tipper Restraint and Transport Guideline

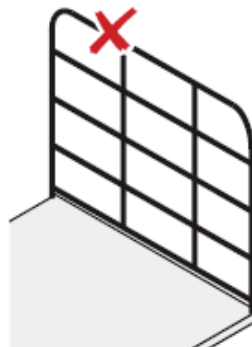
### Load Configuration:

#### Vehicle sides, headboard and tail gate:

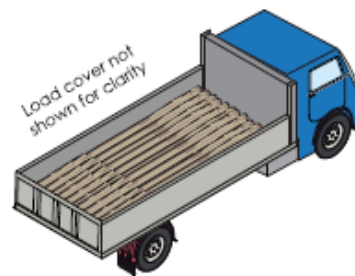
- ✓ Vehicle must have engineered sides, tail gate and headboard (whether fixed or removable) designed for the weight of the load.
- ✗ Do not load without a tail gate. The tailgate is required to contain rearwards movements and provide structural reinforcement to the sides.
- ✓ Ensure tailgate is secure and clips are secured.
- ✓ Headboards should extend to the height of the load such that it protects the cabin.
- ✓ Headboards and tail gates should not contain gaps larger than the transported material (logs or processed wood).
- ✗ Pipe gates must not be used as headboards, side gates or tail boards as they lack sufficient strength.



Engineered  
Headboard



Pipe Gates



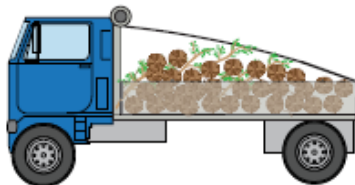
Load cover not  
shown for clarity

Fixed side walls must be  
engineered to meet the  
performance standards  
of the HVNL 2018

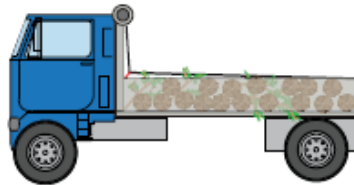
## Light Truck (Fixed or removable sides) and Tippers Restraint and Transport Guideline

### Positioning of load:

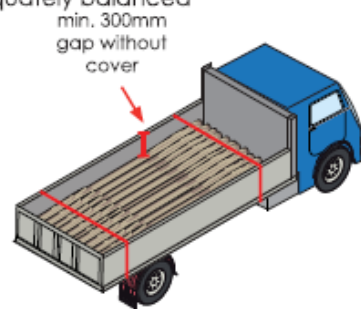
- ✗ Do not fill above side wall height or rear gate (whichever is the smallest).
- ✓ Cargo nets or covers must be used where lashings do not prevent all objects from sliding, rolling or bouncing over the sides.
- ✓ Recommended minimum gap from top of side wall down to top of load is 300mm if a cargo cover is not used.
- ⚠ Do not overload axles when loading logs but ensure it is adequately balanced



✗ Do not overfill as load cover will not stop sideways movement

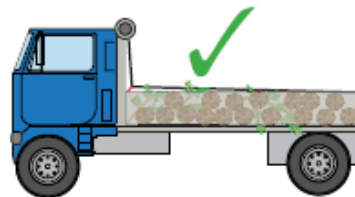


✓ Fill in tray is below side walls height and is covered by load cover



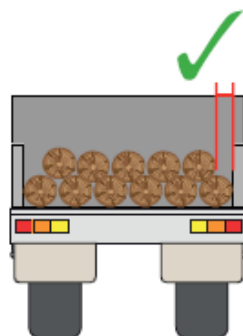
### Load Movement:

- ✓ The maximum gap between the load (logs or processed wood) and the side walls or headboard/tail gate is 300mm.
- ✓ When otherwise secured, the gap between the side walls, headboard or rear gate should be blocked to prevent movement (e.g. use off-cuts, chocks or pins).



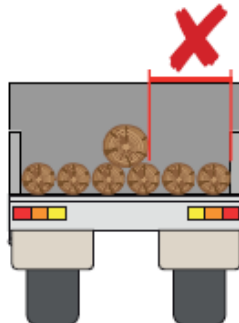
### Load Restraint:

- ✓ Cargo nets or covers must be used where lashings do not prevent all objects from sliding, rolling or bouncing out of the tray.
- ✓ Ensure load covers are used with tie downs straps along the edges to minimise gaps and stop logs bouncing out.

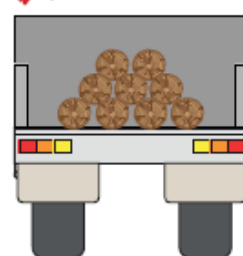


Maximum  
300 mm

Minimise  
gaps



✗ Distribute logs  
across the trailer



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## 12. Appendix 2: Utility vehicles and trailers restraint and transport guideline



### Utility Vehicles and Trailers Restraint and Transport Guideline

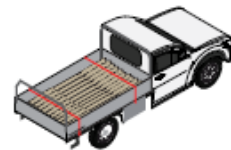


#### Guideline:

This document provides guidance on minimising transport risk of forestry products for operations using utility vehicles and trailers (transporting loads up to 1.5 tonnes respectively).

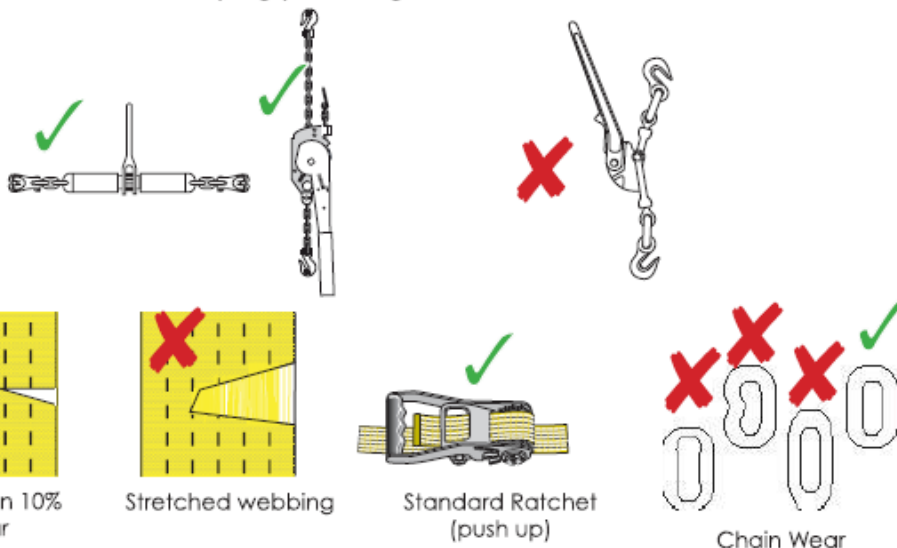
Transport of logs and other forest products (dependant on vehicle type) is also subject to the requirements of:

- Victorian Road Management Act 2004.
- Manufacturing guidelines for the vehicle/trailer



#### Load Restraint Equipment:

- ✓ 50mm or 75mm webbing, conforming to AS/NZS4380.
- ✓ 8mm minimum transport chains, tensioned with load binders, conforming to AS/NZS4344.
- ✓ Ausbinders, turnbuckle ratchet, web-dog binders or similar are a suitable alternative to dogs provided a minimum pre-tension of 750kg can be achieved.
- ✓ Cargo nets must be rated to a minimum 150 kg capacity.
- ✓ Equipment must be in good working order.
- ✓ Inspect the lashings for wear on a 12 monthly basis.
- ✗ Webbing straps worn more than 10% of their width should be discarded.
- ✗ Chains with excessively damaged, worn or bent links must not be used.
- ✗ Over center level binders (dogs) are a high risk and are not to be used.



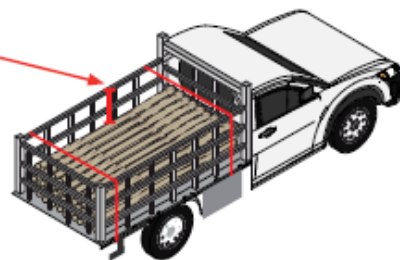
## Utility Vehicles and Trailers Restraint and Transport Guideline

### Load Configuration:

#### Utility vehicle sides, headboard and tail gate:

- ✓ Load must be contained on all sides (hinged or unhinged side walls and tail gate). Taller loads require a headboard to protect the cabin when higher than the cabin window.
- ✓ Hinge side walls are suitable for smaller loads (less than 500kg). Hinged sides must be reinforced with at least 2 lashings.
- ✓ For heavier loads on utility vehicles (loads between 500kg and 1.5 tonne), fixed or removable sides must be engineered to contain the load.
- ✓ Headboards and tail gates should not contain gaps larger than the transported material (logs or processed wood).
- ✓ Ensure tail gate and sides are properly secure.

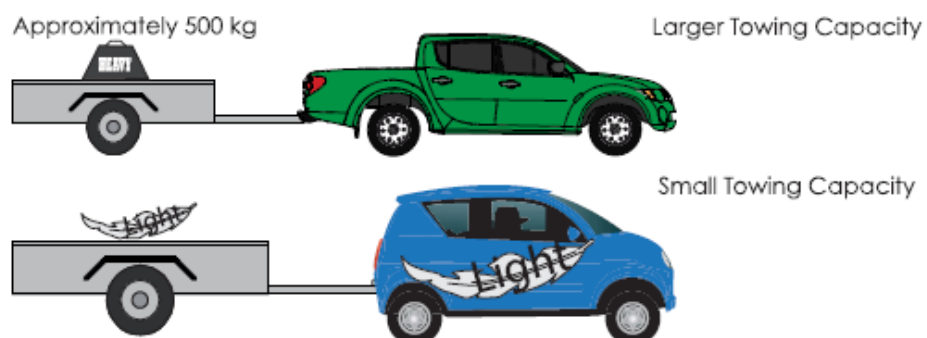
If not applying a cover, ensure there is a gap of min. 300mm between load and top of gate



#### Trailers:

- ✓ Loads must comply with the manufacturers specifications.
- ✓ Trailer brakes (hydraulic or electric) must be functioning and must not be locked-out.
- ✓ Tail gate/front gate must be secured to meet the applied loads.
- ✓ Trailers with hinged sides are suitable only for smaller loads (less than 500kg) and must be reinforced with a least 2 lashings unless otherwise specified by the manufacturer.
- ✓ Trailer mass/ load must take into consideration the capacity of the towing vehicle.

Example:



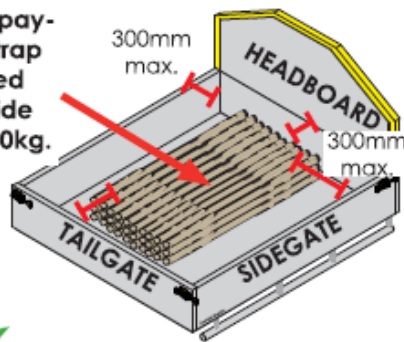
⚠ Small cars may not have 0.75t towing capacity. (Check vehicle handbook)

## Utility Vehicles and Trailers Restraint and Transport Guideline

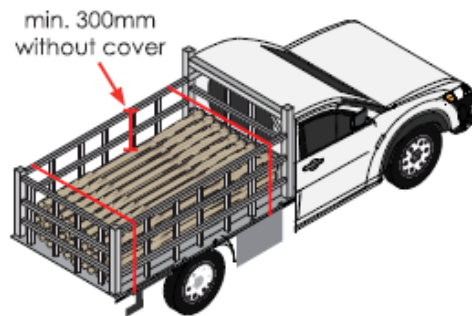
### Positioning of load:

- Do not fill above the lowest side of the vehicle or trailer (including front and rear walls).
- Cargo nets or covers must be used where lashings do not prevent all objects from sliding, rolling or bouncing over the sides.
- Recommended minimum gap from top of side wall down to top of load is 300mm without a cargo cover.
- The maximum gap between the load (logs or processed wood) and the side walls or headboard/tail gate of the vehicle or trailer is 300mm.

Maximum payload for strap reinforced hinged side gates is 500kg.



Total gaps less than 300 mm in all directions

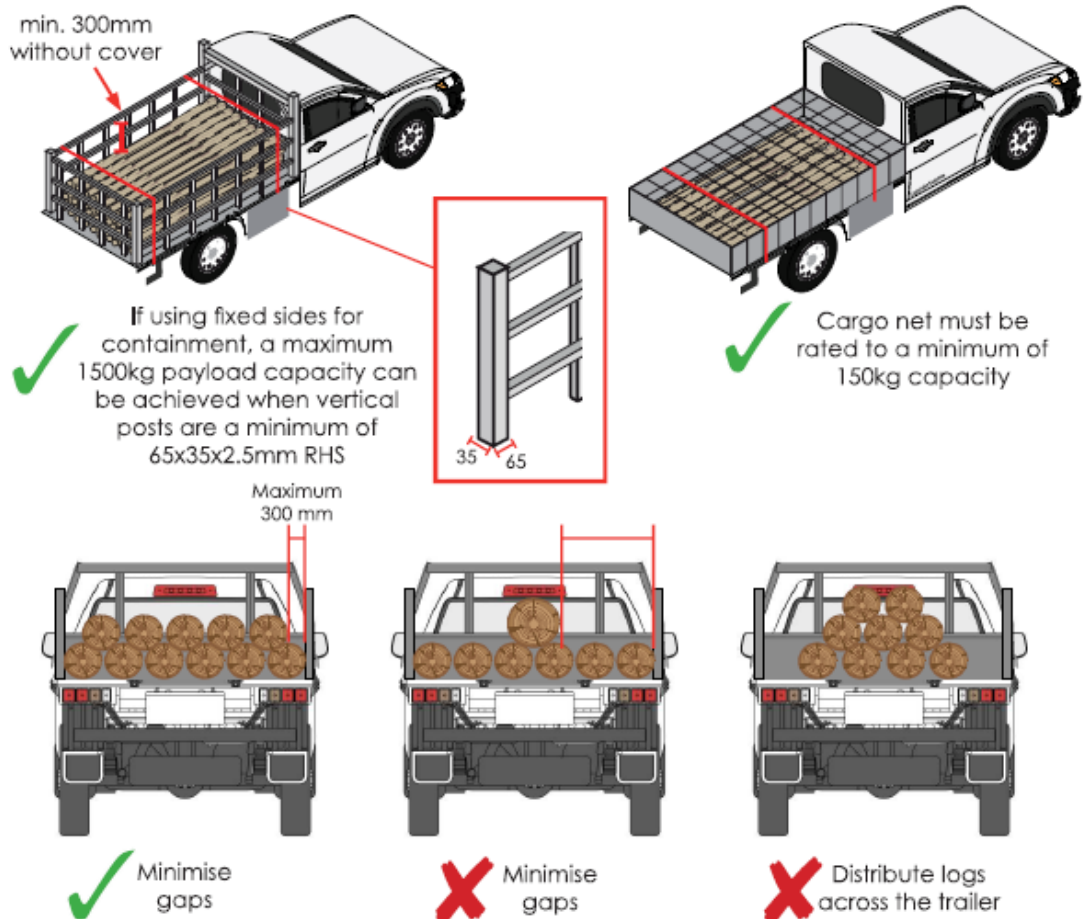


- The gap between the side walls, headboard or rear gate should be blocked to prevent movement (e.g. use off-cuts, chocks or pins).

## Utility Vehicles and Trailers Restraint and Transport Guideline

### Load Restraint:

- ✓ Block loads using Headboards, Side gates, Tailgates or other product.
- ✓ Cargo nets must be rated to a minimum 150 kg capacity.
- ✓ Cargo nets or covers must be used where lashings do not prevent all objects from sliding, rolling or bouncing out of the tray.
- ✓ Use at least two (2) webbing straps to reinforce hinged side gates.
- ✓ Maximum payload for strap reinforced hinged side gates is 500kg.



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### 13. Appendix 3: Light truck (flat bed) log recovery restraint and transport guideline



## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline



#### Guideline:

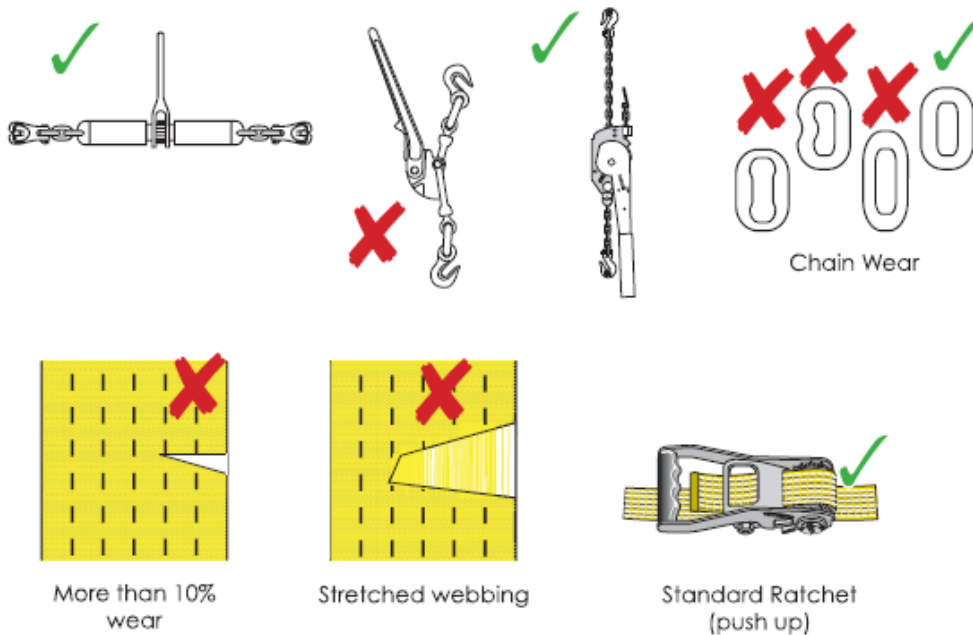
This document provides guidance on minimising transport risk of forestry products (unprocessed) for operations using light flatbed trucks (carrying up to 8 tonne loads).

Transport of logs and other forest products (dependant on vehicle type) is also subject to the requirements of:

- Heavy Vehicle National Law Act 2018.
- Forestry Log Haulage Code of Practice March 2020. (Unprocessed timber and for loads above 8 tonnes)
- Victorian Road Management Act 2004.

#### Load Restraint Equipment:

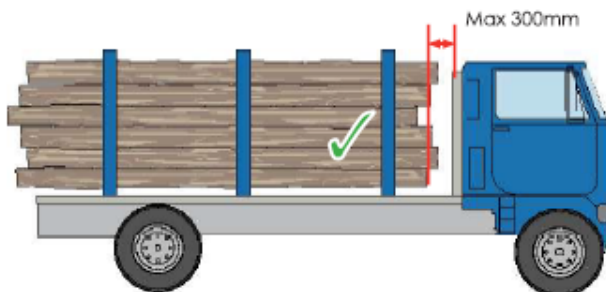
- ✓ 50mm or 75mm webbing, conforming to AS/NZS4380.
- ✓ 8mm minimum transport chains, tensioned with load binders, conforming to AS/NZS4344.
- ✓ Ausbinders, turnbuckle ratchet, web-dog binders or similar are a suitable alternative to dogs provided a minimum pre-tension of 750kg can be achieved.
- ✓ Equipment must be in good working order.
- ✓ Inspect the lashings for wear on a 12 monthly basis.
- ✗ Webbing straps worn more than 10% of their width should be discarded.
- ✗ Chains with excessively damaged, worn or bent links must not be used.
- ✗ Over center level binders (dogs) are a high risk and are not to be used.



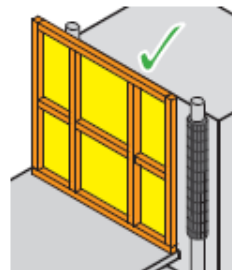
## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline

### Blocking

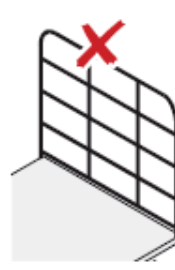
- ✓ Block loads using Engineered Headboards or other suitable surface that will accept in transit forces.
- ✓ Ensure load does not extend over the height of the headboard.
- ⚠ A load is considered to be blocked if **the majority of the logs** are a maximum 300mm away from the blocking surface.
- ✗ Pipe gates must not be used as headboards.
- ⚠ Tailboards may be used but are not a requirement for restraining log loads up to 8t.



Max 300mm gap to blocking surface  
for blocked loads



Engineered  
Headboard

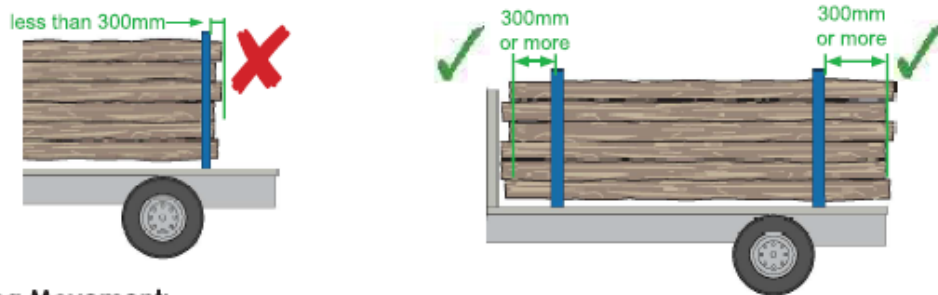


Pipe Gates

## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline

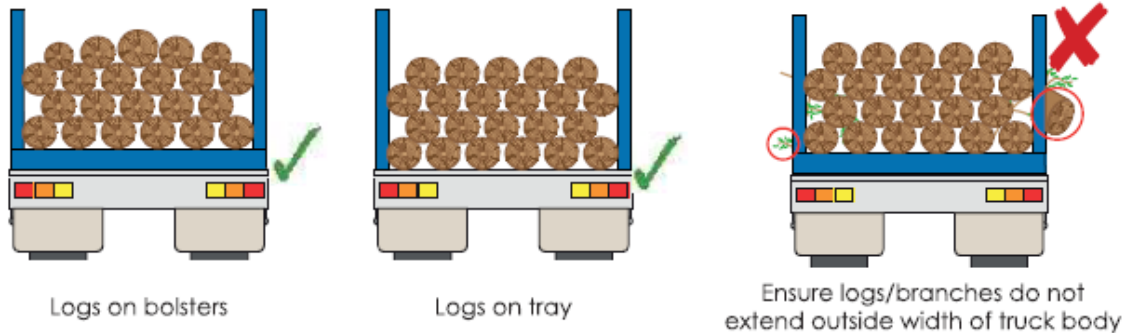
### Load Configuration:

- ✓ Ensure all logs extend at least 300mm past stanchions.



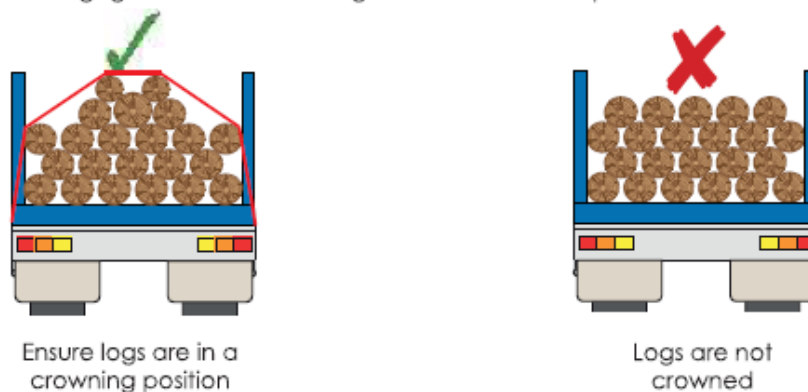
### Minimising Movement:

- ✓ Loading logs on stanchions.
- ✓ Retention of bark on logs.
- ✓ Transport logs when dry.
- ✓ Use of tailboards/back gates.





### Crowning Loads:

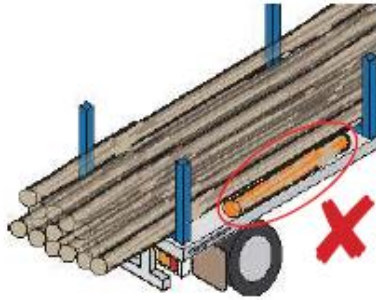
- ✓ Ensure logs are in a crowning position.
- ✗ Do not load logs greater than half a log width above the top of the stanchions.



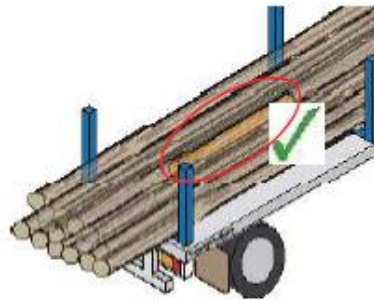
## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline

### Mixed Length Loads:

-  Short logs not contained by stanchions should be minimised.
-  Place short logs in the centre of the load.




Short logs must not be positioned towards the outside of the stack

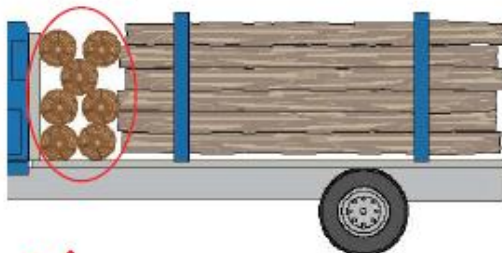



Position short logs centrally in the stack of logs.

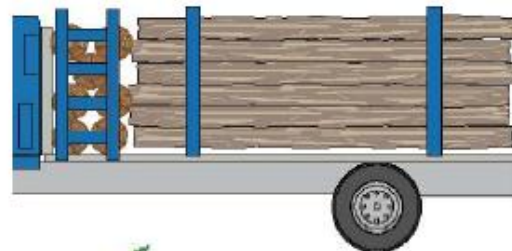



### Sideways Loads:

-  Ensure the loads are contained within side gates or secured as per requirements used for lengthways loads.








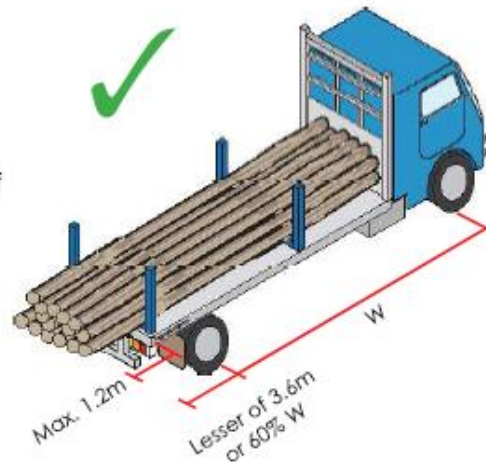
 Ensure transverse logs are contained within side gates



 Block logs from sideways movement

### Rear Overhang:

-  Rigid truck width to not exceed 2.5m.
-  Rigid truck length to not exceed 12.5m.
-  Rigid truck height to not exceed 4.3m.
-  Maximum allowable rear overhang is whichever of the smallest of:
  - 1.2m from the end of the tray.
  - OR
  - 3.6m from the centre of the rear axle group.
  - OR
  - 60% of the wheelbase from the centre of the rear axle group.
-  For overhangs not easily visible, a bright coloured flat (red, yellow or combination) is required.



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## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline

### Truck and Trailer Combinations Greater than 8t GCM:

- ✓ For all activities using a float type trailer and/or heavy trucks, refer to the log haulage code of practice administered by AFCA.

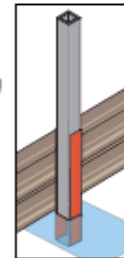


Refer to code of practice for effectively restraining logs on heavier vehicles.

### Stanchions requirements:

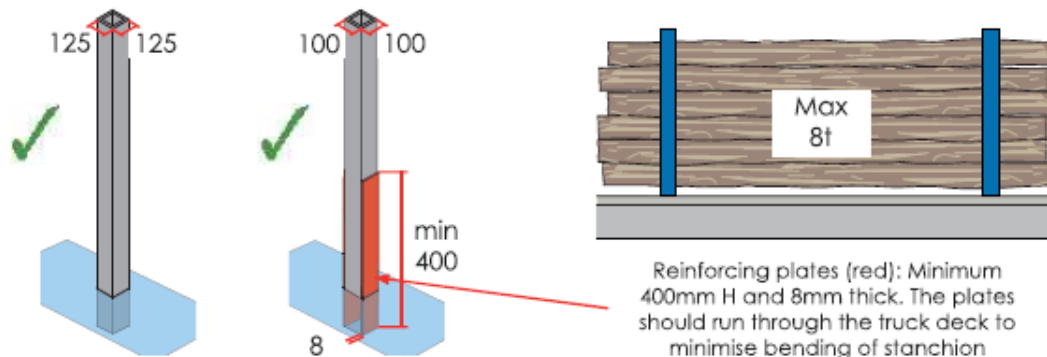
- ✓ Stanchions/Pins must be in good working order.
- ✓ Minimum requirements can be found below.
- ✓ If minimum requirement cannot be met, Apply support beams at base of stanchion

Stanchion with reinforcing plates to improve bending strength of stanchions



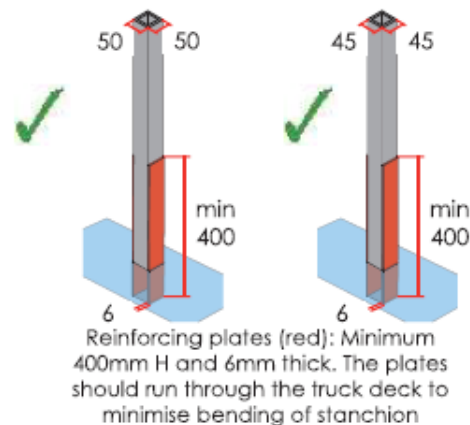
### Two (2) stanchions for loads 2-8 tonnes:

- ⚠ Stanchions up to 8t must have a thickness of at least 6mm.
- ⚠ If stanchion is smaller than 125x125x6mm, apply two (2) steel plates on either side of stanchion to reinforce (minimum 400mm H x 8mm thick).



### Two (2) stanchions for loads up to 2 tonne:

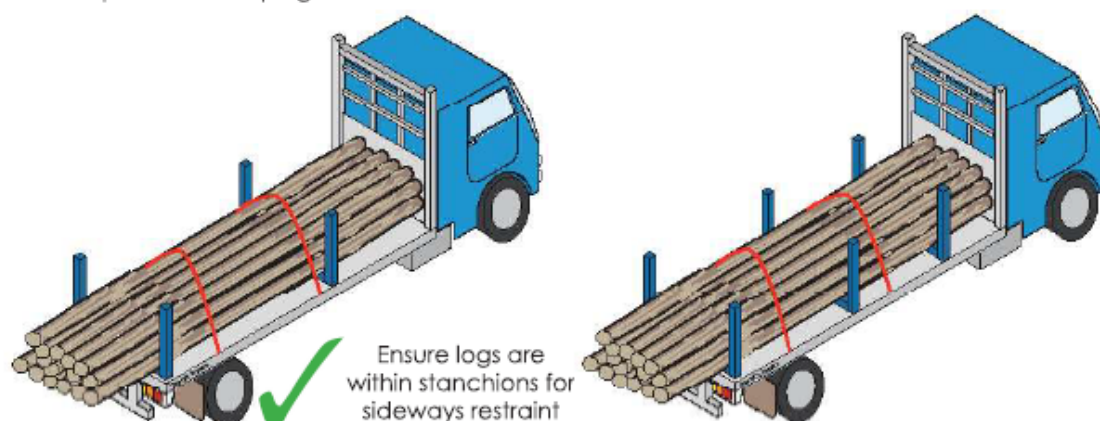
- ⚠ Loads up to 2t must have stanchions with a thickness of at least 2.5mm and apply two (2) steel plates on either side of stanchion to reinforce (minimum 400mm H x 6mm thick)



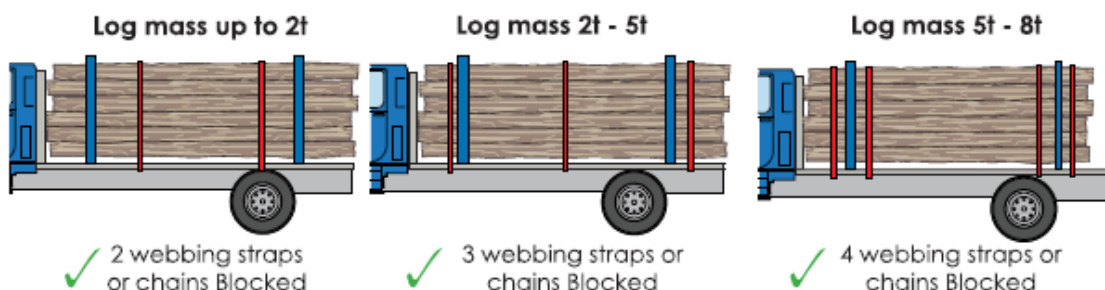
## Light truck (Flat bed) Log Recovery Restraint and Transport Guideline

### Load Restraint:

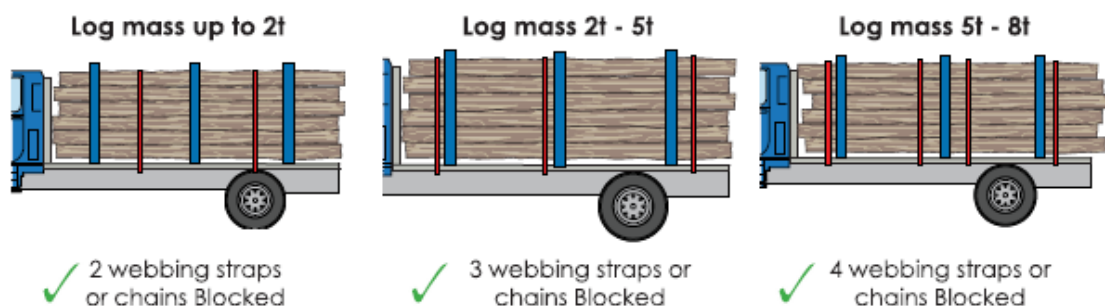
- ✓ Minimum of 2 stanchions per group of logs on each side of the vehicle.
- ✓ Ensure stanchions are suitable to restrain the bay mass - typical stanchion section sizes are provided on page 4.



### Minimum 2 Stanchions per side:



### 3 Stanchions per side:



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